

(m) Rooftop Mechanical Equipment. Rooftop mechanical equipment will be screened in a manner which enhances the overall architectural design, and rooftops will be of a color that reduces glare and other types of visual impact on the adjacent residentially developed hillsides.

(i) Rooftops should incorporate features which soften rectilinear forms and effectively screen mechanical equipment from view.

(2) Transportation Guidelines.

(a) Vehicular.

(i) ~~Roadway~~Street Configurations.

(A) ~~Roadways~~Streets that are above existing grade should be designed in a manner to reduce visual impact of pavement area such as using landscaping or berms.

(B) Encourage alignment of all ~~roadways~~streets to minimize the removal of all existing significant, healthy trees.

(C) Streets should not be wider than four travel lanes with the appropriate number of lanes at intersections between the ~~design area~~district, and areas targeted for integration with the downtown.

(D) Vehicular circulation shall relate the various uses on the site to each other. ~~Roads~~Streets shall be designed to enhance viability of the project components.

(E) Vehicle access to parking areas should be designed to minimize interaction of vehicles with pedestrians.

(ii) Parking – Surface.

(A) Where possible, locate parking behind buildings and away from areas of high public visibility. Landscape and screen surface parking areas visible to the public.

(B) Parking areas should include landscape areas and be designed to minimize long, straight, monotonous rows. The size and location of parking areas should be minimized and related to the group of buildings served.

(C) Parking areas should include a clear circulation network to guide pedestrians to the building entrances.

(D) Visual impact of surface parking areas should be minimized from the SR 520 corridor.

- (E) Landscaping shall be provided to screen surface parking areas and provide transition between the project and surrounding areas particularly when viewed from SR 520, Leary Way and adjacent hillsides.

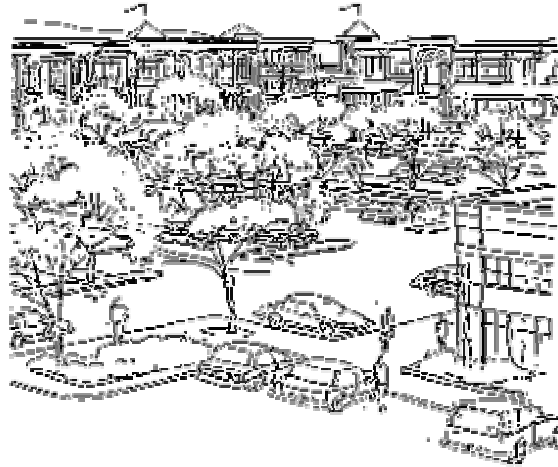


Figure 10

- (F) Parking lots should be landscaped to screen parking and provide visual relief from large asphalt surfaces.
- (G) Pedestrian access from parking areas should be safe and clearly defined.
- (H) Landscaped medians should be provided where access and traffic allow.
- (I) Conflict between pedestrians and automobiles should be minimized by designing streets to provide well-defined pedestrian walkways and crosswalks that reduce vehicle speeds.
- (J) Open space and landscaping should be coordinated and linked wherever possible, particularly in relation to public areas and the pedestrian system.
- (K) To minimize visual impacts where surface parking is utilized, project design should incorporate screening and landscape treatment of surface parking areas.
- (L) Design and locate parking areas in a manner that will break up large areas of parking and encourage shared parking with existing downtown uses.
- (M) Patrons of the retail center shall be allowed to use parking while patronizing other businesses in ~~the Downtown City Center~~. No rules, signage or penalties shall be enacted by Town Center to preclude this parking allowance.

(iii) Parking – Structured.

- (A) Structured parking should be designed to avoid undifferentiated planes. The scale of parking structures should be modulated by interruptions of the facades, setbacks, and lowering the first level below the existing grade (where the water table allows) to reduce total height.
  - (B) Facades of parking structures should include a landscape treatment in addition to architectural screening from the SR 520 corridor.
  - (C) Parking structures shall have landscaping around the perimeter which will correspond to adjacent land uses and activities. Landscaping shall include, but not be limited to, a combination of shade trees, evergreen trees, shrubs, groundcovers, deciduous native and ornamental shrubs, and vines to further screen the structures.
  - (D) The top floor of parking structures should include landscape screening in areas such as along the cornice and on the deck, either by trees or a screening trellis treatment.
  - (E) Provide walkways in parking floors which have curbs or other barriers to protect from vehicular intrusion.
  - (F) For security, pedestrian routes should be visible and avoid enclosed, hidden areas. Emergency call boxes should be available.
  - (G) At least 50 percent of the parking provided for the entire site should occur in parking structures. The ratio of minimum structured parking shall be maintained for all phases of development of the ~~Mixed Use Retail~~ Town Center Mixed-Use area and the Parkway Office areas ~~Park~~.
  - (H) Parking structures should be enclosed with retail or office uses on the exterior or where this enclosure is not feasible, the visual impact should be softened with landscaping or screening.
- (b) Pedestrian.
- (i) Linkages.
    - (A) Link proposed development to walkways, trails, and bicycle systems in the surrounding area by connecting and lining up directly to existing linkages, closing gaps and treating crossings of barriers (such as the railroad, Bear Creek Parkway, and driveways) with special design treatment, minimizing barriers, designing with consistent materials, widths and locations, and providing safe, easy and clearly identifiable access to and along the linkages. Safe, convenient and attractive connections to Marymoor Park, the Sammamish River Trail, and the Bear Creek Trail system should be provided.

- (B) The sidewalk system shall be emphasized with landscape treatments to provide readily perceived pedestrian pathways through and around the ~~Mixed Use Center Design Area~~ Town Center district.
- (ii) Sidewalks.
- (A) When extending an existing sidewalk, the new walkway should meet current standards and regulations where there is sufficient right-of-way, and should be constructed of a material and dimension which is compatible with and improves upon the existing character.
  - (B) Sidewalks should meet similar standards to those of the approved pedestrian linkage system.
  - (C) Paving of sidewalks and pedestrian crossings should be safe and constructed of a uniform material that is compatible with the character of the ~~design~~ district. The private use of sidewalk rights-of-way areas may be appropriate for seasonal cafe seating or special displays.
  - (D) Encourage alignment of new sidewalks to minimize the removal of all existing significant, healthy trees.
- (iii) Arcades, Colonnades, Canopies.
- (A) In areas of high pedestrian activity, devices for protection from weather are encouraged. Consistent treatment within a single area is also encouraged in order to provide a strong identity of space. (See Figures 1, 5, 7 and 9.)
  - (B) Buildings should be designed to provide for weather and wind protection at the ground level. Buildings fronting sidewalks should provide pedestrian weather protection by way of arcades, colonnades, or canopies a minimum of 48 inches in depth. The elements should be complementary to the building's design and design of contiguous weather protection elements on adjoining buildings. Materials and design should engender qualities of permanence and appeal.
  - (C) Awnings or sunshades should be harmonious to the design of the building to which attached, and should be in keeping with the character of the building. Materials should be durable, long lasting, and require low maintenance. Back-lit awnings are discouraged.
- (iv) Enclosed Malls. The design of enclosed malls should allow pedestrians through access during hours of retail center operation while still maintaining security.

- (v) Trails – Pedestrian. Special design treatment and appropriate safety features should be afforded the pedestrian trail crossings at public rights-of-way and at the BNSF right-of-way ~~Burlington Northern Railroad~~ tracks.
- (vi) Trails – Bicycle. Facilities for parking and locking bicycles should be provided and be readily accessible from bicycle trails.
- (vii) Trails – Equestrian. Width of the trail should be adequate for two riders side-by-side in order to avoid earth compaction and vegetation deterioration. Equestrian trails should be separate from pedestrian and bicycle trails. ~~Equestrian trails do not have to be constructed until off-site linkage is constructed to the proposed development.~~
- (viii) Plazas/Pedestrian Malls. Plazas, pedestrian malls and other amenity open spaces shall be developed that promote outdoor activity and encourage pedestrian circulation between the Mixed-Use Town Center district and the balance of the ~~City Center area~~ Downtown.

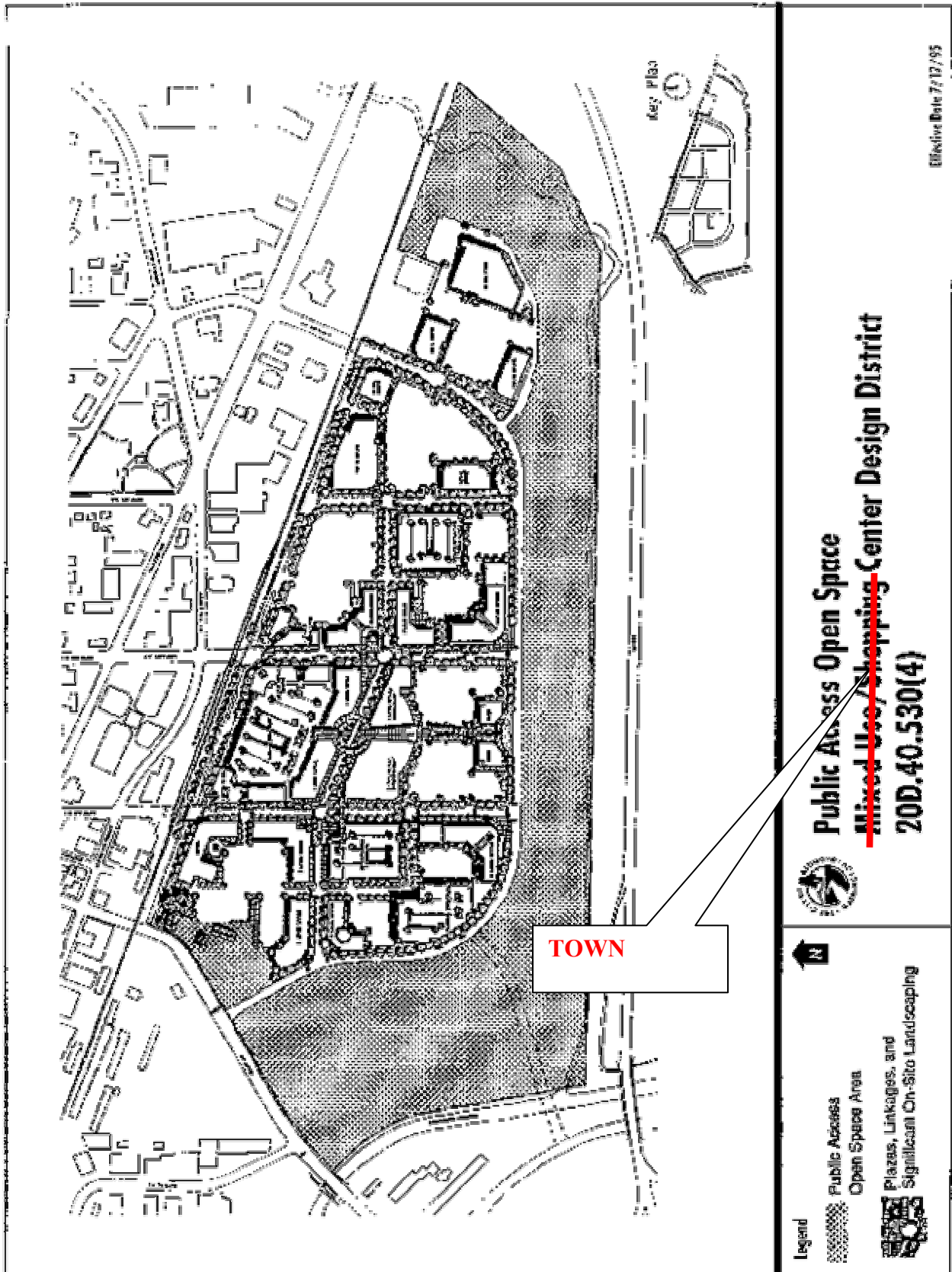
### (3) Landscape Guidelines.

- (a) Urban Landscape Treatment. Building entries, primary vehicular entries and building perimeters should be enhanced with landscaping which could include ornamental vines, groundcovers, shrubs and/or trees selected for their screening, canopy, spatial enclosure and seasonal variation.
- (b) Site Furnishings. Benches, kiosks, signs, bollards, waste receptacles, street vending carts, water fountains, lighting standards, perch walls, sidewalks, pathways, trails and special water features should be designed to be compatible elements of like materials and design.
- (c) Perimeter Landscaping. Landscaping on the perimeter of the site will create a transition between the project and the surrounding area.
- (d) Landscaping on Streets. Landscaping on streets should be simplified to allow adequate visibility from automobiles to businesses.
- (e) Trees, Plants and Flowers. The use of potted plants and flowers as well as street trees are encouraged, but should not impede pedestrian traffic.

### (4) Open Space Guidelines.

- (a) Tree Retention and Open Space Landscaping. Preserve existing natural features, particularly healthy mature trees and stream courses.
  - (i) Preserve a minimum of 100 percent of all trees within the public access open space as identified in the Public Access Open Space Area Plan. This area includes the cluster of trees along the east side of Leary Way for the purpose of preserving the corridor's green gateway image, and the healthy trees along the Bear Creek and Sammamish

River corridors. Trees that cannot be retained due to approved street and/or utility construction shall be replaced with native nursery stock of similar or like variety at a one to one ratio, with tree sizes in accordance with RCDG 20D.80.20-080, pursuant to a landscape plan approved in conjunction with site plan review. Trees removed as a result of construction activities which are intended to be preserved shall be replaced per RCDG 20D.80.20-080. Replacement trees shall be located in the immediate vicinity as is practical.



- (ii) Minimize new grading in this area.
  - (iii) Install landscape screening between this open space area and adjacent parking areas.
  - (iv) Encourage passive recreation including a walking trail, bicycle trail, seating and rest areas, pedestrian lighting, and site furnishings. Connections should be provided to the Justice White House, ~~Mixed Use Retail~~ Town Center Mixed-Use area, Marymoor Park, Sammamish River Trail system and other open space areas.
  - (v) The “soft edge” landscape treatment to the south of Town Center along Bear Creek shall provide for a true transition between the natural, riparian area of the creek to the more urban Mixed Use ~~Design-Retail~~ Area.
  - (vi) The informal nature of the west, south and east portion of the site should be maintained by retaining native materials and random planting of compatible plant materials consistent with the ~~City Center~~ Downtown Neighborhood ~~Program~~.
- (b) Justice White House/Saturday Market. The areas around the Saturday Market and Justice White House shall be retained as open space. Areas at the Justice White House should encourage active and passive recreation. These areas should connect to other open spaces, trails, and the Mixed Use Retail area.
- (c) Sammamish River. Open space should be retained along the Sammamish River and enhanced by:
- (i) Providing grade separation for trails at all appropriate and feasible locations;
  - (ii) Making connections to other open space zones;
  - (iii) An ongoing storm water outflow monitoring program for private drainage systems. ~~shall be designed and, upon approval by the Technical Committee, be implemented by the applicant.~~ The monitoring program shall consider specific contaminants which may likely be present in the runoff and shall be revised periodically as appropriate.
- (d) Bear Creek. Open space along Bear Creek should be retained and enhanced by:
- (i) Encouraging passive recreation areas and activities and discouraging active recreation.
  - (ii) All storm water swales and recharge areas should be integrated with the natural environment.
  - (iii) Protecting vegetation of the riparian habitat in this zone by limiting access to the creek to designated access points.
  - (iv) Providing connections to Marymoor Park, the Sammamish River, ~~Mixed Use Center~~ and Town Center and other open spaces.



- (v) Facilities within this area should include a pedestrian pathway, bicycle path, equestrian trail when required, passive water access area, seating, and site furnishings.
  - (vi) An ongoing storm water outflow monitoring program for private drainage systems. ~~shall be designed and, upon approval by the Technical Committee, be implemented by the applicant.~~ The monitoring program shall consider specific contaminants which may likely be present in the runoff and shall be revised periodically as appropriate.
  - (e) Public Access Open Space. Public access open space should be retained, enhanced and made available for public use in this ~~district design area~~ as shown in the Public Access Open Space Area Plan.
    - (i) At least 44 acres shall be preserved by easement to the City or controlled by other methods that would permanently assure the open space to the City. This downtown public access open space shall serve as a visual amenity and passive recreation open space.
  - (f) Open Space Acreage. Public access open space as shown in the Public Access Open Space Area Plan shall include a minimum of 44 acres. This will include natural areas inclusive of the floodway, and the areas around the Justice White House and the Saturday Market.
- (5) Signage Guidelines.
- (a) Sign Kiosks. Sign kiosks, which provide direction to groups of tenants, are encouraged as a means for discouraging numerous individual directional tenant signs and providing clear directions. These kiosks shall be located in high traffic areas and be consistent with the design theme of the development.
  - (b) Symbols and Colors. International symbols and colors should be used whenever applicable.
  - (c) SR 520 Signage. Signage for the subject proposal shall comply with the SR 520 Corridor Signage Policy adopted by the City Council.
  - (d) Signs will be evaluated according to the overall impact, size, shape, texture, lettering style, method of attachment, color, and lighting in relation to use, the building and street where the sign will be located, and the relationship of the sign to other signs and other buildings in the vicinity. Adherence to or enhancement of sight lines both parallel and perpendicular to the sidewalks will be considered. The primary reference will be to the average pedestrian's eye-level view, although views into or down the street from adjacent buildings should be an integral feature of any review.

- (e) Signs should be simple, clear and direct. Extruded aluminum or plastic signs are not appropriate. Careful use of neon may be acceptable.
- (f) Signs should not hide or obscure the architectural elements of the building.
- (g) Signs attached or hung from the arcades, colonnades, and canopies perpendicular to the sidewalk should be encouraged.
- (h) Projecting, double-faced or three-dimensional signs may be allowed if they are integrated into the overall streetscape and do not destroy sight lines. Symbolic three-dimensional signs such as a shaving mug, barber pole, pawn shop symbol, or other symbols illustrating the product being sold on the premises are acceptable provided they meet other sign guidelines. When possible, signs should reflect the character and the use within the structure.
- (i) Sign lighting should be energy efficient.

(6) Lighting Plan.

- (a) A lighting plan ~~and program shall be provided and approved~~, which encourages nighttime pedestrian movement between the adjacent commercial areas, particularly Leary Way and 166th Avenue NE ~~shall be maintained~~.
- (b) Parking lot and security lighting shall be screened by buildings and other means to minimize light intrusion onto the Sammamish River and Bear Creek.
- (c) Street lighting should relate in scale to the pedestrian characters of the area. The design of the light standards and luminaries should enhance the design theme. (Ord. 1993)

**~~20D.40.130 Leary River Bend District Design Area.~~**

**~~20D.40.130-010 Intent.~~**

- (1) Leary Way, between the Sammamish River and the ~~Burlington Northern railroad tracks~~ BNSF right-of-way, should remain as a “green gateway” to the City of Redmond. Setbacks from Leary Way should be adequate to preserve significant stands of trees on the west side of the road to preserve the “green gateway” image.
- (2) Signage on Leary Way for traffic control, directional information, and business identification should be kept at a minimum, in both size and number. (Ord. 1993)
- (3) Bear Creek Parkway, between Redmond Way and Leary Way should also be treated as an urban gateway to the Downtown neighborhood, evolving with a more traditional ground floor store front character as it redevelops over time. The setback from the property line on this portion of Leary Way shall be at least 100 feet where there is an existing grove of

~~significant trees (six trees within a quarter-acre area). No parking shall be allowed within the setback. No clearing, grading permit or site plan approval shall be granted until the approval authority determines that the existing trees will be preserved to the maximum extent possible within the setback.~~

- (4) Ground floor building facades along the westerly Bear Creek Parkway extension, as it is envisioned to extend from Redmond Way on the west to Leary Way on the east, should be pedestrian friendly and pedestrian scaled, with narrow storefronts along the sidewalk edges, awnings for pedestrian comfort, and ample storefront windows.



- (5) Developments on larger interior lots, with relatively little existing street frontage, should be designed as mixed-use villages with internal pedestrian friendly streets and pathways serving mixed-use buildings.

#### **20D.40.130-020 Design Criteria.**

##### **(1) Building Orientation and Access.**

- (a) Vehicle access and parking should be designed to minimize curb cuts along Leary Way and to minimize the loss of existing trees within the ~~district~~design area.
- (b) Where significant trees exist between the street and building sites, driveways from the primary streets to on-site parking areas should be designed in a tree-lined parkway manner, complementing, preserving, and taking advantage of remaining stands of trees.
- (c) Building orientation should complement any remaining stands of trees.

##### **(2) Building and Site Design.**

- (a) The setback from the property line on this portion of Leary Way shall be at least 100 feet where there is an existing grove of significant trees (six trees within a quarter-acre area). No parking shall be allowed within the setback. No clearing, grading permit or site plan approval shall be granted until the approval authority determines that the existing trees will be preserved to the maximum extent possible within the setback.
- (b) Site layout for individual parcels within the ~~design area~~district should be designed to provide reciprocal vehicular and pedestrian access to and from adjoining lots in order to achieve a unified circulation plan which minimizes curb cuts on Leary Way and Bear Creek Parkway, and provides pedestrian connections between uses in the ~~design area~~district.
- (~~bc~~) New landscaping should be designed to complement and reinforce any remaining groves of trees within the ~~design area~~district. Large trees and shrubs, both evergreen and deciduous, should be used to complement the natural character of this ~~design area~~district.
- (~~ed~~) Buildings and site layout should be designed to complement the mixed-use village-retail character of the adjoining Old Town ~~Design Area~~District, which includes features such as narrow ground floor storefronts on pedestrian-oriented streets, shops fronting on street sidewalks, and pedestrian-scale architecture. On larger lots with relatively little existing street frontage, building and site layout should be designed to extend the traditional downtown street grid into the site, with storefronts fronting on internal streets and paths.
- (e) Column and bay spacing along street fronts should be provided approximately 30 feet apart in order to maintain a pedestrian-oriented scale and rhythm of store fronts.
- (~~df~~) Pedestrian connections from buildings within ~~the this design district area~~ should be provided to the adjoining Old Town and ~~Mixed Use Shopping Town~~ Center Design ~~Areas~~districts. (Ord. 1993)

MERGED WITH  
20D.40.130

**20D.40.135 ~~River Bend Design Area (Reserve)~~**

**~~20D.40.135-010 Intent:~~**

- ~~(1) Provide visibility and access from automobiles while also providing adequate landscaping and screening.~~
- ~~(2) Create and maintain a lower scale commercial and service oriented design area. (Ord. 1993)~~

**~~20D.40.135-020 Design Criteria:~~**

**~~(1) Building Orientation and Access:~~**

- ~~(a) Orient buildings toward the streets and away from adjacent residential uses.~~
- ~~(b) Service areas and rear entrances shall be oriented away from adjacent residential uses, public open spaces, and primary streets.~~

~~(c) Vehicular access should be minimized on the primary arterial, and shared access is encouraged.~~

~~(2) Building and Site Design:~~

~~(a) Parking lots should be landscaped to screen parking and provide visual relief from large asphalt surfaces.~~

~~(b) Parking lot and security lighting shall be screened by buildings and other means to avoid light intrusion onto the adjacent river and residential uses.~~

~~(c) Buildings shall be sited to ensure maximum land use compatibility between adjoining and adjacent uses, while minimizing impacts on each other. (Ord. 1993)~~

**20D.40.140 RESERVE North Point and Carter Design Areas.**

MERGED WITH 20D.40.145

~~20D.40.140-010 Intent:~~

~~(1) These residential design areas are intended to provide areas of greater residential density within the downtown in order to create a better balance between jobs and housing, and to provide a variety of housing choices within close proximity to goods and services.~~

~~(2) Provide opportunities to develop moderate density housing in small residential neighborhoods that are in close proximity to schools, shopping, parks, and services. (Ord. 1993)~~

~~20D.40.140-020 Design Criteria:~~

~~(1) Building Orientation and Access:~~

~~(a) Buildings should be oriented toward the primary streets.~~

~~(b) Primary entrance to buildings shall be oriented to the street. See RCDG 20C.40.105, City Center Pedestrian System.~~

~~(c) Yards of ground-oriented units fronting on streets should be designed to indicate a separation between public and private spaces, while also providing maximum territorial view of the street front from the units' private open spaces.~~

~~(d) Vehicular access to sites from streets should be minimized. Alley access should be utilized where possible and feasible to minimize curb cuts along street fronts, whereby maintaining available street front parking.~~

~~(2) Building and Site Design:~~

- ~~(a) Buildings shall provide focus and articulation of primary entrances orienting toward streets.~~
- ~~(b) Buildings and private usable open spaces shall be appropriately setback from property lines to provide ample light, air, and privacy for the on-site tenants and adjacent neighbors.~~
- ~~(c) Buildings should be designed to provide maximum amounts of usable open space on site for the residents and to provide front yards complementary to the size and scale of the developments.~~
- ~~(d) Parking shall be enclosed within subterranean or semi-subterranean parking whenever possible to provide maximum amounts of usable open space on parking decks and minimize building heights.~~
- ~~(e) Building facades and rooflines shall be articulated to provide visual interest and relief as well as to promote long-term neighborhood and community pride.~~
- ~~(f) Open exterior corridors fronting and visible from streets and public spaces shall be minimized in use and length in order to minimize their negative visual impacts on surrounding streets and properties. They should be placed on the interior of developments whenever feasible.~~
- ~~(g) On-site parking, both within structures and outdoors, shall be screened from public streets.~~
- ~~(h) Common usable open spaces and yards shall be adequately lighted and visible from units within the developments to ensure security and safety of the residents and their guests. (Ord. 1993)~~

**20D.40.145 North Point, Carter and East Hill Design Area Districts**

**20D.40.145-010 Intent**

- (1) These residential districts are intended to provide a variety of housing choices. These districts are intended to provide comfortable and attractive living environments with a variety of housing types, and only limited mixed-use opportunities, in developments that include more typical residential features such as front yards, landscaping, and ground related patios and porches.
- (12) The intent for the East Hill district is to allow Downtown's original single-family neighborhood to evolve from its existing collection of houses into a denser multi-family neighborhood that honors the character of the original single-family houses and neighborhood character. New multi-family development, as well as conversions of the existing single-family residences to commercial uses should maintain and enhance the



~~residential look and character of the district. Create an area which allows a variety of uses side-by-side that maintain a residential look and feel in terms of design and character, in order to promote and maintain a comfortable and attractive living environment while also allowing limited amounts of nonresidential uses next to residential uses.~~

- ~~(23) In the East Hill District, ensure that conversions of single-family residences to commercial uses are done in a manner that does not negatively affect the “residential” character of the existing structures and front yards. Provide opportunities to create mixed use residential, office/retail, and residential-only developments within a neighborhood context and scale. (Ord. 2051; Ord. 1993)~~

**20D.40.145-020 Design Criteria.**

**(1) Building Orientation and Access.**

- (a) Buildings should be oriented toward the ~~primary~~ streets.
- (b) Primary entrance ~~to of~~ buildings shall be oriented to the street, and ground floor units that have patios along the street should be designed with walkways from the patios to the street sidewalk in order to establish a “front door” relationship with the streets. See example below. See RCDG 20C.40.105, City Center Pedestrian System.



- (c) Yards of ground-~~oriented~~ floor residential units fronting on streets should be designed to ~~indicate~~ provide a separation between public and private spaces, while also providing maximum territorial view of the street front from the units' private open spaces.

- (d) Vehicular access to sites from streets should be minimized. Alley access should be utilized where possible and feasible to minimize curb cuts along street fronts, ~~whereby maintaining available street-front parking and to maximize the amount of available curbside parking.~~

(2) Building and Site Design.

- (a) Buildings shall provide focus and articulation of primary entrances along the street front. ~~Primary entrances shall not be hidden from the street or accessed via the side yard. orienting toward streets.~~
- (b) Buildings and private usable open spaces shall be appropriately setback from property lines to provide ample light, air, and privacy for the on-site tenants and adjacent neighbors.
- (c) Residential buildings should be designed to provide maximum amounts of usable open space on site, or nearby, for the residents and to provide front yards complementary to the size and scale of the developments.
- (d) Parking shall be enclosed within subterranean or semi-subterranean parking whenever possible to provide maximum amounts of usable open space on parking decks and minimize building heights.
- (e) Building facades and roof lines shall be articulated to provide visual interest and relief as well as to promote long-term neighborhood and community pride.
- (f) ~~Office and mixed~~ Mixed use buildings should be designed with a residential character, in terms of building materials, roof design, fenestration, and front yard landscaping.



- (g) Open exterior corridors fronting and visible from streets and public spaces shall be minimized in use and length in order to minimize their negative visual impacts on



surrounding streets and properties. They should be placed on the interior of developments whenever feasible.

(h) On-site parking, both within structures and outdoors, shall be screened from public streets. Surface parking spaces shall not be located between the building and the street.

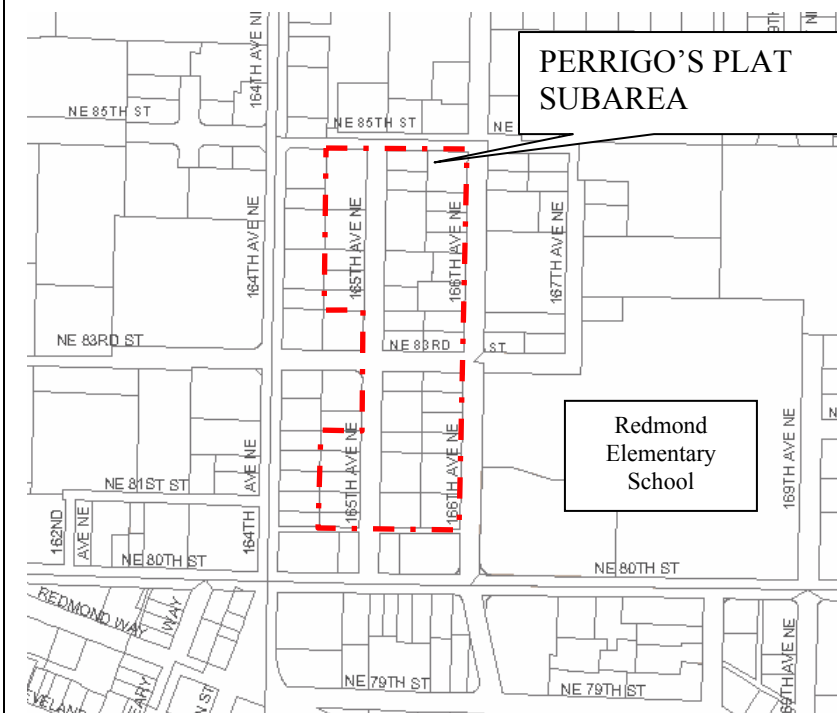
(i) Common usable open spaces and yards shall be adequately lighted and visible from private spaces within the developments to ensure security and safety of the tenants, residents, and their guests. (Ord. 1993)

(j) Conversions from single-family residences to commercial uses shall not significantly alter the residential character of the original single-family structures or front yards. Additions and or modifications to these structures for commercial use shall maintain or enhance the residential look of the property.

**20D.40.145-030 Perrigo's Plat Subarea.**

1. Intent. The intent of this section is to retain the quaint and inviting character of this subarea, which includes a variety of building styles and features, generous front yards, sense of quiet and peacefulness, large trees along the street, and unique bungalows, by establishing design criteria that encourage new development to emulate and enhance the traditional residential character of the subarea.

2. Design Criteria. In the Perrigo's Plat subarea of the East Hill district, as shown below, these additional standards shall apply.



- (a) New developments, redevelopments, and remodeling projects shall emphasize features typically found in neighborhoods with older bungalow, craftsman, and Victorian styled homes, including but not limited to, pitched roofs, dormer windows; front doors with porches or stoops oriented to the street; chimneys and double-hung or multi-paned windows.
- (b) New development, redevelopment, and remodeling projects shall use high proportions of visually appealing, high quality, exterior building materials, such as brick, stone, and masonry, on at least the base of the building. The cladding materials of the body shall also be durable, attractive, and complementary to the early 1900's craftsman/Victorian residential style including brick, horizontal siding, and wood (appearing) shingles.
- (c) For developments consisting of multiple aggregated lots, building widths should generally be no longer than 100 feet wide to ensure that building mass remains consistent with the existing neighborhood character and scale.
- (d) New development, redevelopment, and remodeling shall be designed with front yards characteristic of single-family neighborhoods, including street trees along the street, with trees and landscaping in the front yard.

**20D.40.150 ~~City Center~~Old Town District Historic Design Subarea Standards.**

**20D.40.50-010 ~~Purpose~~Intent.**

The intent of this section is to preserve the existing Historic Landmarks in the district, ensure that development and redevelopment complement the historic character and function of Downtown's original pedestrian retail area, and to maintain or enhance the historic integrity of the Leary Way corridor. These standards are established to maintain and enhance the traditional "main street" character of the neighborhood, which includes continuous pedestrian-oriented storefronts and pedestrian-scaled streetscapes along the streets. The purpose of this division is:

- (1) To preserve the existing Historic Landmarks within the District;
- (2) To maintain or enhance the historic integrity of the Leary Way corridor and;
- (3) To ensure new development and additions or other remodeling within the City Center Historic Design Subarea is complimentary to and in context with the existing historic fabric.

**20D.40.150-020 ~~RESERVE~~Scope and Authority**

The guidelines in this section shall apply only within the City Center Historic Design Subarea as shown on the map entitled City Center Historic Design Subarea. Citywide and City Center design standards shall also apply to this area. Where the Citywide or City Center design standards, or sign standards conflict, the Historic Design Subarea guidelines shall prevail.

**20D.40.150-030 Site Design**

- (1) Intent. The core of this district developed between the late 1800's to the mid 1900's encompassing a variety of periods. The intent of site design standards within the ~~City Center~~

~~Historic Design Subarea~~ Old Town district is to ~~ensure that~~ maintain the historic patterns of land development and general character of the district that was created during this timeframe ~~are maintained.~~

(2) Design Criteria.

- (a) Building setbacks. Setbacks shall reflect historic patterns. ~~Commercial buildings~~ Buildings shall front on the public sidewalk. ~~Setbacks of residential structures from the public sidewalk should be minimized. Narrow areas of lawn or landscaping may be acceptable between entrances.~~ Structures should be built to the side lot line or the side setback should be 5 feet or less not including access width. Rear setbacks may allow for a narrow alley access, for service entrances or for several stalls of parking. Additions or other remodels are encouraged to match this pattern when feasible.
- (b) New structures should incorporate structured parking or mechanical stacked parking systems. Visible structured parking shall not front along the sidewalk edge except to allow placement of parking entrances. Structured parking should preferably be underground or in backs of structures. Surfaces masking parking shall continue design rhythms, colors, materials, and trims used in the other parts of the structure. Surface lots should be located to the rear of structures.

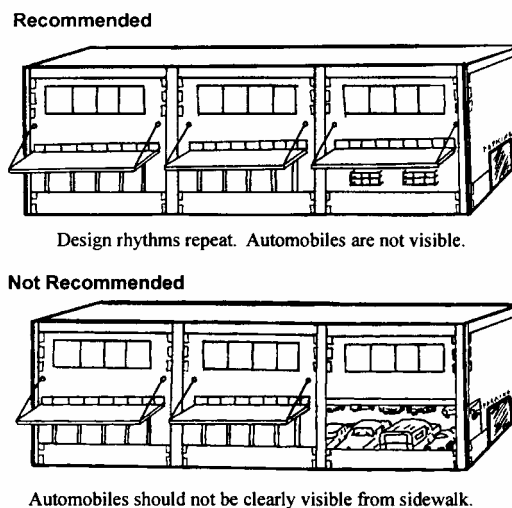


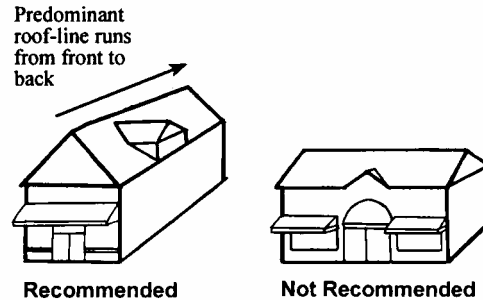
Figure 1. Parking Design

**20D.40.150-40 Building Design**

- (1) The intent of building design standards within the ~~City Center~~ Old Town district ~~Historic Design Subarea~~ is to ensure that Historic Landmark buildings maintain their prominence within the district and to ensure the character of the original downtown core is maintained. These building design standards shall apply to all new structures. ~~Whenever it is feasible and~~

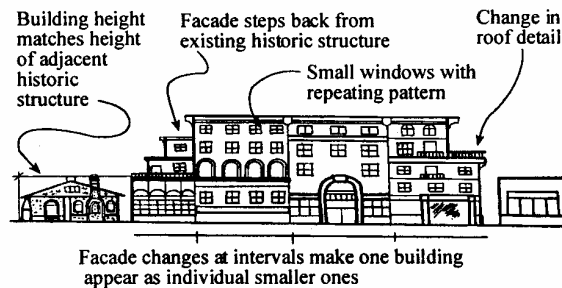
~~in context with the existing structure.~~ These standards will also apply to additions or other remodeling whenever it is feasible and in context with the existing structure.

- (2) Building orientation.
- (a) Entrances shall front towards the sidewalk.
  - (b) For peaked roofs, primary roof ridgelines or roof ridgelines predominant to the viewer shall run from front to back.

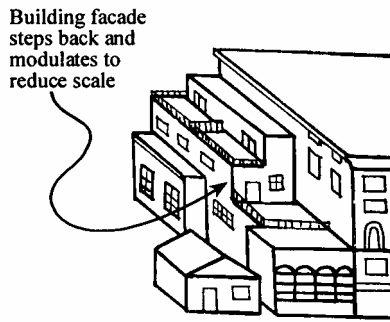


**Figure 2. Roof Ridgelines for Peaked Roofs**

- (3) Building scale. New construction should be in scale with existing historic patterns. In cases where the scale of the building is larger, techniques should be used to break up the scale of the building to ~~match-complement~~ existing patterns. These techniques can include ~~variance~~ variations in roof height, ~~Vertical-vertical~~ columns to break up facades, changes in roof or parapet detail, use of smaller repeating window patterns, use of fascia on the facade, façade articulation, and stepping back or modulating of upper stories.

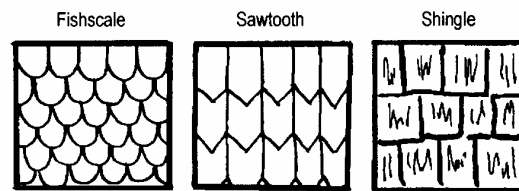


**Figure 3. Building Scale**



**Figure 4. Methods to reduce scale**

- (4) Building design, details, and materials.
- (a) Buildings should incorporate vernacular architectural styles from the periods reflected in the district.
  - (b) Buildings should incorporate materials similar to those used on existing historic structures. This includes brick, stucco that is used in the art deco style, wood, and stone. Use of modern materials is acceptable to the extent that it differentiates the new structures from historic ones without substantially changing the visual character reflected in the district.

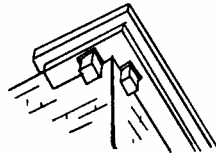


Wood sidings common to the historic period of Redmond. These sidings were often used individually or in combination with clapboard or board and batten. These sidings or reasonable facsimiles in modern materials are encouraged.

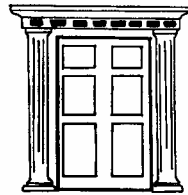
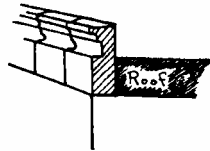
**Figure 5. Illustration of Common Wood Siding materials**

- (c) For 1 to 1-1/2 ~~one to one and one-half~~ story structures, a false front is allowed on peaked roofs.
- (d) Hipped roofs are discouraged unless they are in context with the period of architecture reflected in the district.
- (e) Additions and other remodeling to existing buildings should closely pattern-match or complement the historic architectural styles reflected in the district. ~~Otherwise, additions and other remodeling should be complimentary to the pattern of the district.~~
- (f) Buildings shall incorporate details prevalent in the architecture reflected in the district.

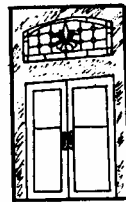
Cornice Details such as this are encouraged. Use of molding or brackets also enhance historic character.



Parapets and parapet details such as this are encouraged. The detail of the parapet is enhanced when it differs in material or color.



Use of columns, or pilasters can enhance door detail. Use of such detail as pediments or architraves for doors is also encouraged.

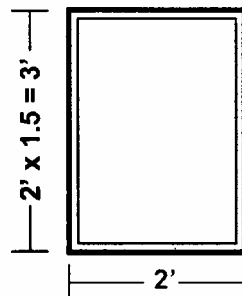


Transoms above doors and use of decorative glass are details that can enhance historic character.

**Figure 6. Details encouraged to enhance Historic character**

- (g) Details such as parapets and bands, windowsills, and doorframes shall consist of alternate-contrasting materials or color to enhance detail.
- (h) Size of individual windowpanes or false panes shall not exceed the smaller sizes typical of early period glass separated by mullions and rails. For windows above the first story, the height or vertical dimension shall be at least one and one-half (1 to 1-1/2)- times the width or horizontal dimension or mullions or stiles should separate the panes to create the sense that the dimension of the vertical element exceeds the horizontal one.

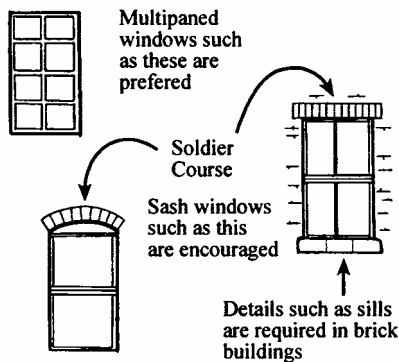
Example of Ratio Requirements:



Window Height must equal or exceed Width by 1 1/2 times.

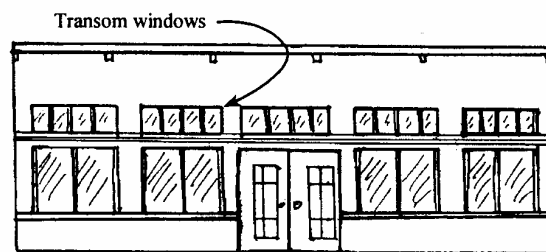
**Figure 7. Window Dimensions above First Story**

- (i) Building fenestration shall consist of numerous individual windows or larger windows having multi-panes to avoid large areas of solid surface.
- (j) For brick, stone, and art-deco stucco structures, upper story windows shall be punched or recessed except for bay windows.
- (k) Details such as soldier course or solid lintels and sills shall be used around windows in brick, and stone structures.



**Figure 8. Multipaned Windows and Sill and Lintel treatments**

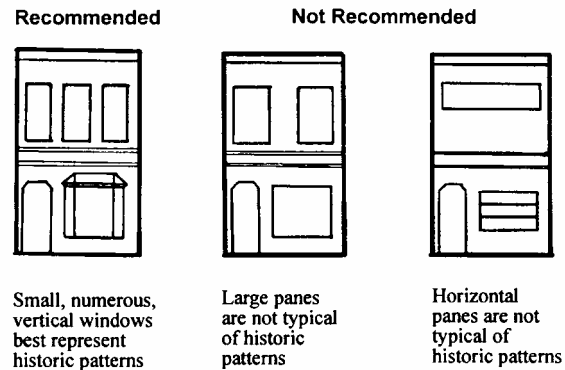
- (l) Windows shall not start at floor level and shall repeat historic patterns that allowed views to the interior of a storefront.
- (m) Transom windows are encouraged.



Window shall not start at floor level. This illustration shows a typical starting point for many historic buildings.

**Figure 9. Transom Windows and Windows Above Floor level**

- (n) Preferred window dimensions and type are illustrated in Figure 10.



**Figure 10. Preferred Window Dimensions and Types**

- (o) Recessed entrances are encouraged for storefronts. Porches, courtyard entrances, or stoops are encouraged for ground floor residential units where allowed buildings.

(5) Building Color.

- (a) New buildings, additions, and other remodels may contrast with that of existing buildings to the extent that use of color still allows a Historic Landmark building to remain prominent within the district.
- (b) Preferred colors are those that reflect the historic patterns of the district, however other colors, particularly on non-historic structures, will be allowed if they are complimentary to surrounding structures.

**20D.40.150-50 Signage and Awnings**

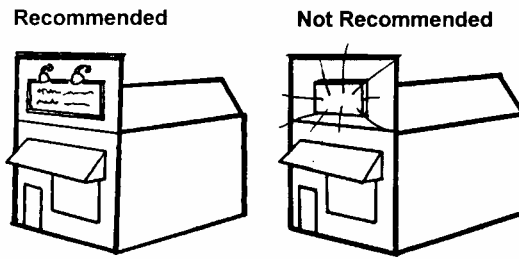
- (1) Intent. ~~The intent of sign standards within the City Center Historic Design Subarea is to~~ To ensure that all buildings in the Old Town district have signage compatible with or complementary to the Historic Landmark, the period of the structure, or with the general period reflected in the district.

(2) Design Criteria.

- (a) Size of signs ~~will~~ shall comply with ~~the sign RCDG~~ 20D.160 Signs unless either compliance would be detrimental to the integrity of a Historic Landmark or when a design concept is more in keeping with the architecture reflected in the district.

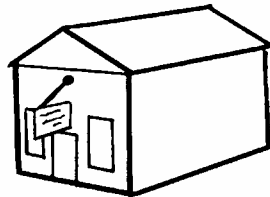
- (3b) Preferred signage. Window signage, painted signage on wooden facades, wood signs, signs on awnings, signs lit by sources other than the sign itself, or decorative signs hung perpendicular to the building façade are encouraged. Wood facsimile products may be an acceptable substitute. Use of backlit plastic wall signs, extruded aluminum, changing message, or other newer technology signs should not be allowed. Use of neon signs should be limited to window signs or art deco styled buildings.





Use of painted signs lit by separate source such as illustrated here are encouraged. Highly modern signs such as this backlit plastic sign are strongly discouraged.

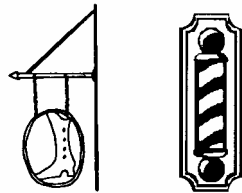
**Figure 11. Preferred Signage**



Signs hung perpendicular to front facade are encouraged.

**Figure 12. Perpendicular Signage**

- (4c) Use of signs with logos illustrating trades is encouraged.



Trade logo signs are encouraged

**Figure 13. Typical Trade Logo Signs**

- (5d) Awning design, color, and lettering should be compatible with or ~~complimentary~~ complementary to the awning styles, colors, and lettering that are typical of the periods reflected in the district.
- (6e) For awnings, preferred styles are wedge awnings, bullnose entrance awnings, and flat decorative metal awnings anchored to the structure.

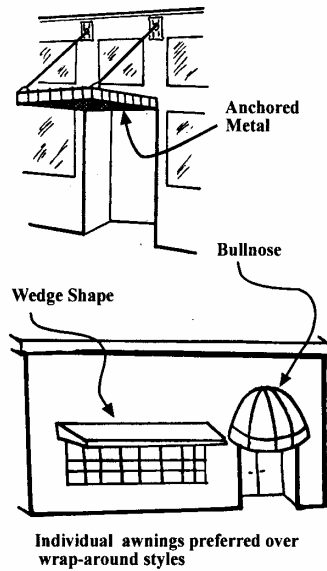


Figure 14. Preferred Awning Styles

(7f) For wedge style awnings, a narrow awning style is preferred.

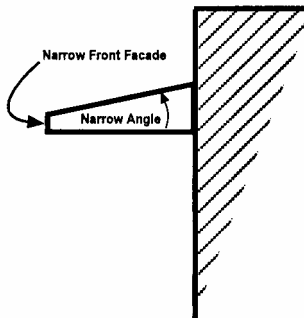


Figure 15. Preferred Style for Wedge shaped Awnings

(8g) Individual awnings over window and entrance awnings are preferred to wrap around awnings.

(9h) It is preferable for awnings that the lettering and logo does not dominate the awning façade unless the lettering style and size is in keeping with the historic period of the structure.

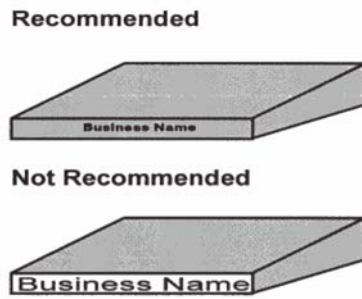


Figure 16. Name & Logo should not Dominate awning

(10i) The use of backlighting on fabric awnings is strongly discouraged.

#### **20D.40.150-060 Pedestrian/Customer Elements.**

(1) Intent. The Old Town district should be emphasized as a pedestrian shopping and gathering environment, with comfortable and attractive sidewalks, plazas, informal seating areas, and pedestrian amenities that are consistent with the historic character of the district.

#### **(2) Design Criteria.**

- (a) Trees, Plants, and Flowers. The use of potted plants and flowers as well as street trees are encouraged, but should not impede pedestrian traffic.
- (b) Street Lighting. Street lighting should relate in scale to the historic character of the area. The design of the light standards and luminaires should enhance an historic theme.
- (c) Street Furniture. Public seating, trash receptacles, and informational/directional kiosks should be of uniform design and be provided throughout Old Town. Creation of informal gathering places that are integrated appropriately with the streetscape is encouraged.
- (d) Sidewalks. Paving of sidewalks and other pedestrian rights-of-way should be constructed of a uniform material that is compatible with the historic pedestrian character. The private use of sidewalk rights-of-way areas may be appropriate for seasonal cafe seating or special displays.



FIGURE 1



FIGURE 2



FIGURE 3



FIGURE 4



FIGURE 5





FIGURE 6

